



### 3. GUIDELINES FOR HARDSTANDINGS IN FRONT GARDENS

Under the Scheme of Management, proposals to form hardstandings for off street parking, resurfacing of an existing drive with a different material or hardstanding and any other alterations that would change the appearance of a property, require prior written approval.

Applications regarding improving accessibility to a property will be supported. However planted front gardens are an important feature of the Estate and good quality planting can create cooler air temperatures, help filter pollutants, reduce noise and create wildlife habitats. Adding hard landscaping can have a detrimental environmental impact as well as a negative impact on the streetscape.

Consideration must be given to the impact on the environment and the streetscape and the need for off -street parking should be demonstrated. The effects of climate change mean that heavy rainfall events are more frequent and drains in urban areas such as Dulwich are unable to cope. Creating large areas of hard landscaping in front and rear gardens adds to the risk of local flooding. Where a hardstanding is acceptable, the type of surface materials and associated landscaping must be approved and where necessary, permissions must be obtained from the local authority, particularly if changes are required to the pavement or kerb. The local authority will take into account



*Original landscaping and coherent materials are distinctive features of the Estate*

road safety and will be responsible for creating the dropped kerb. If you live on a road owned and maintained by the Estate, permission to alter the pavement or kerb would need to be obtained from the Estates Department.

The following factors will be considered when a permit application is made for a hardstanding or for the resurfacing of an existing hardstanding:

Applications which would result in the loss of a substantial proportion of front garden, will not be approved. At least 50% of the total garden area should be planted with well-maintained soft planting. If you are using gravel or 'grass-crete' this cannot be included in your calculation. We stipulate this as a minimum percentage of planting to keep our community green.

Proposals should use permeable (or porous) surfacing, and allow water to flow to a lawn, border, French drain or soakaway, rather than relying on the public mains drainage. Further information on permeable surfacing can be found here.

The material which forms the hardstanding, drive and paths must be in sympathy with the design of the property and streetscape. Modern group developments like the Whytefield Estate were carefully landscaped when they were originally built. The consistent pallet of materials and details created cohesive developments which set them apart from others at the time. These materials and details should be conserved, but alternative permeable materials such as resin bound surfacing can match these original materials. Details of the environmental credentials of the material chosen should be submitted as well as photographic samples. Where relevant please photograph the proposed material next to the original.

Traditional and unique properties should have sensitive natural materials such as gravel, stone, bonded gravel, 'hoggin' or granite sets. Heritage block paving and resin bound gravel materials will also be considered but samples should be provided. Hardstandings will not be approved where it is considered they would have a negative impact on the setting and architectural character of a group of houses or the streetscape.

The width of the entrance created on the front boundary for vehicle access should be kept to a minimum to reduce the visibility of parked cars on driveways. Generally, this is 3.5m - 4m. Where possible, the front garden should be designed with evergreen planting which screens the parked vehicle from the road.

### 3. GUIDELINES FOR HARDSTANDINGS IN FRONT GARDENS CONT.

The approved planting scheme must be maintained to provide adequate screening of a parked vehicle and to ensure the green character of the streetscape is maintained. When plants and shrubs die, the freeholder is expected to plant replacements.

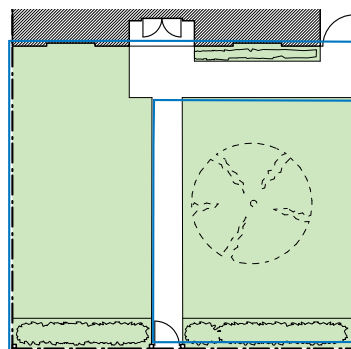
Maintaining and planting new trees and shrubs and building ponds encourages biodiversity and enhances local wildlife habitats. Planting also helps reduce the levels of carbon dioxide in the air. This is called carbon sequestration. Maximising the space in which you plant perennial plants and minimising hard landscaping can significantly improve your carbon footprint. Plants need water to survive and as our climate gets drier, we should consider how we can reduce water consumption with measures such as improving soil, creating shade, planting drought resistant species and collecting rainwater.

When designing your front garden bear in mind that trees of amenity value should not only be retained but their roots must to be protected. Care should be taken when resurfacing and laying services to avoid damaging significant tree roots. Surface roots of trees in the top layer of soil are important to tree health and should also be considered. An arboricultural report should be provided with hand-dug root surveys, and root protection method statements should be provided.

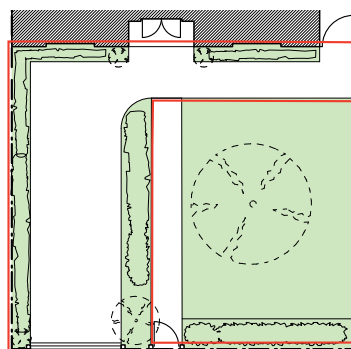
Electric vehicle (EV) charging points can be installed to front gardens and further details can be found in Guideline 16.

#### SUITABLE ADAPTATION OF A FRONT GARDEN

Existing plan



Proposed plan



#### Exceptions to the 50% planting guideline

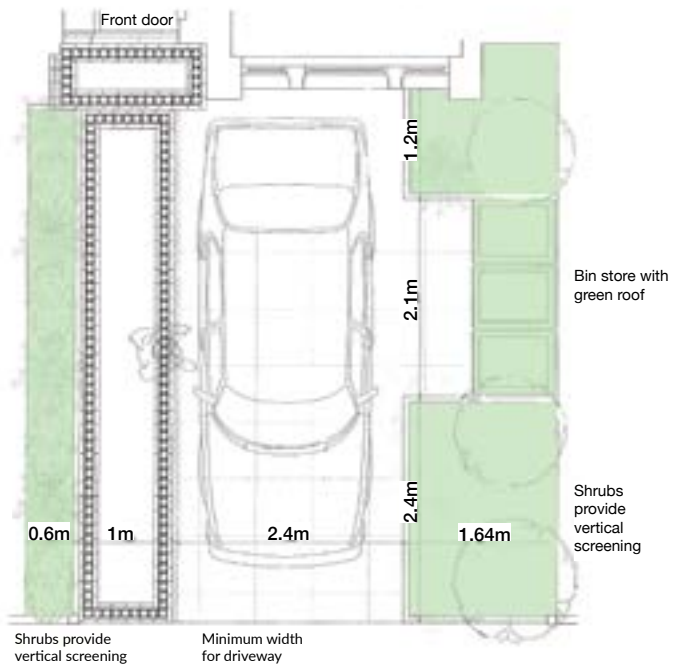
This will only be considered on Turney Road, subject to detail, where most of the terrace can provide a minimum area of 28%. A template for this street has been approved by the residents' association, The Dulwich Society and Scheme of Management and can be found at [www.turneyandburbage.org.uk](http://www.turneyandburbage.org.uk).

Further exceptions may be considered if the following criteria have been met.

- The property is on a red-route and it is essential to be able to turn a car around on the driveway to avoid reversing into traffic.
- The maximum area possible is planted and the surface material is permeable.
- The green character of the streetscape would be enhanced overall, for example through vertical planting of trees and shrubs and evergreen screen planting.
- Parked cars must be screened from view from the street as far as possible.
- The landscape design is a biodiverse enhancement and of very good quality.

### 3. GUIDELINES FOR HARDSTANDINGS IN FRONT GARDENS CONT.

#### SUITABLE PLAN FOR SOME TURNEY ROAD HOUSES



Please note that planning approval may be required from your local Council - see its Planning Portal for details.

[https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment\\_data/file/7728/pavingfrontgardens.pdf](https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/7728/pavingfrontgardens.pdf)

#### WHAT TO INCLUDE IN YOUR APPLICATION

- Accurate dimensioned drawings to a scale of 1:50.
- Changes proposed to the front boundary (walls, hedges, gates and other features) must be shown in elevation on the drawings.
- The dimensions of the hard landscaped area should be included on the drawings together with details of the materials proposed. Photographic samples of the actual materials will be required.
- Details of surface water drainage. Larger applications, (new builds and re-developments) may need to provide a detailed SUDS (Sustainable Urban Drainage System) assessment report as part of the supporting information.
- Details of the planting scheme should also be provided and consideration should be given to biodiversity – further information may be requested if these are not comprehensive.
- Details of any tree work, an arboricultural report with hand dug root surveys, and root protection method statement.

Please note that planning approval may be required from your local Council - see the Planning Portal for details.